

## **NOTES**

1. All local streets, private and public, shall be designed and constructed in compliance with the requirements of this and all other applicable City of Orange Public Works Department Standard Plans, unless otherwise approved by the City Engineer.

The use of local streets shall be limited to local interior residential streets not carrying through traffic.

The design of all local streets shall be in accordance with City Standards and shall be inspected by the Public Works Department, with a standard inspection fee to be paid.

The use of local streets as a device for permitting inadequate design will not be allowed.

- 5. The entire width of the traveled way of the local street shall be owned by or dedicated to the City as a permanent sewer and public utilities easement, i.e., the easements granted to the City shall take precedence and have prior rights over any other easement.
- Where streets are constructed to such standards that eliminate parking on one or both sides of the street, appropriate signs reading "No Parking at any time" shall be erected where parking has been eliminated.

7. The following design standards shall be used.

#### A. GENERAL

- Where special circumstances exist, deviations from the standards will be subject to approval by the City Engineer after an individual review.
- Streets shall be adequately designed to provide for lane delineation, street sweeping, and drainage control. a standard crown section with standard concrete curb and gutter on both sides will be required.
- 3. Minimum of 5' wide and in compliance with latest ADA access requirements concrete sidewalks shall be provided on all private streets, unless other means of accommodating pedestrian traffic are provided in the development.

Lighting adequate for pedestrian and vehicular safety and sufficient to minimize police problems as approved by the Public

Where there is direct vehicular access to a street from adjacent residential units and it is proposed to reduce the street width by eliminating curb side parking, an equivalent number of street parking stalls shall be calculated by dividing the total linear feet of available street frontage parking by 22 feet on which curb-side parking is eliminated.

Adequate provisions shall be made for trash pick-up and for emergency vehicle access.

7. Adequate signs, conforming with the Orange City Code relating to signing and subject to approval of the City, shall be provided and maintained at all entrances to private streets clearly designating the private status of such streets.

#### B. MINIMUM WIDTHS: CURB TO CURB

1. With parking and driveway on both sides: 36' CURB-TO-CURB

With parking and driveway on one side only: 32' CURB-TO-CURB

These widths shall be increased as required by traffic demands as determined by the City Engineer.

## C. MINIMUM WIDTHS: RW TO RW AND PARKWAY

1. With parking and driveway on both sides: 52' RW-TO-RW (8' parkway)

With parking not permitted on one side and driveway on both sides: 48' RW-TO-RW (8' parkway)

3. With parking not permitted on one side and driveway on one side only: 44' RW-TO-RW (8' parkway on driveway side and 4' parkway without access).

These widths shall be increased as required by traffic demands as determined by the City Engineer. 14' half street width with the approval of the City Engineer may be reduced to 12' on the side without driveway access for street with volume less than 500 Average Daily Traffic.

### D. GRADES

1. Maximum:

10%

Minimum:

1% Provide vertical curves for grade breaks over 0.5%

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PUBLIC WORKS DEPARTMENT

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108

SHEET 2 OF 4

**STANDARD** 

WORKS DIRECTOR

STANDARD LOCAL STREETS

## NOTES: continued

#### E. ALIGNMENT

- Intersections shall occur at right angles whenever possible. Skewed intersections in excess of 15° from a right angle shall not be permitted.
- A minimum centerline tangent distance of 20 feet shall be required between the EC of a curvilinear street
  and the prolongation of the nearest Right-of-Way line of an intersecting street.
- Cul-de-sacs or dead-end streets shall not exceed 600' in length, adequate provision shall be made for the turn-around of trash and emergency vehicles.
- 4. Centerline radius shall be 200' minimum for all streets. Where right angled bends are used in the street pattern, in lieu of the minimum radii required above, widening sufficient to accommodate truck turning movements shall be provided by use of knuckles or other appropriate means.
- 5. Private streets shall be monumented by a registered Land Surveyor or Civil Engineer in the same manner as would be required for the development of a public street.
- 6. Curb return radius shall be 25' minimum.

#### F. STRUCTURAL SECTION

 The minimum structural section shall be 0.5'A.C. over 0.5'A.B. over 0.5' of compacted subgrade. The section shall be increased as required by accepted standards of engineering design in accordance with the results of soil tests on the native subgrade material.

### FOR INFORMATION:

- 1. Private streets shall meet a minimum requirement of 4.5 cfs surface drainage capability.
- 2. All primary sewer and drainage facilities shall be constructed within the paved accessways.
- 3. Parking lots shall have a minimum 4"A.C. over 4"A.B.

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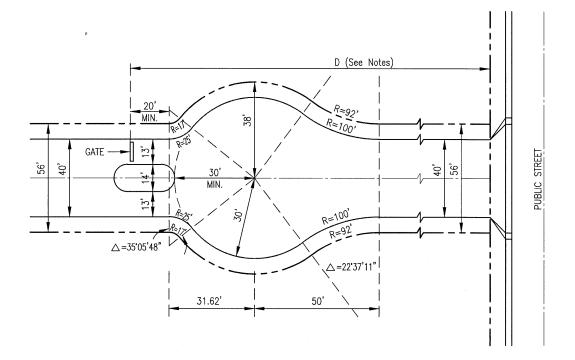
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# PRIVATE STREET ENTRANCE

## NOTES:

- 1. Private streets shall be permitted only as described in the City of Orange Subdivision Code, subject to review and approval by the City Engineer, Community Development Director and Planning Commission.
- Private streets shall provide a paved travel way in conformance with Std. Plan 108. Walkways shall be provided on all private streets in conformance with Std. Plan 108 and 118 unless an alternate pedestrian circulation system is provided meeting the approval of the Engineer.
- Required povement structural section shall be determined by the City Engineer.
   Entryways to private tracts shall be designed to emphasize their private status. Textured concrete or wide flare driveways, guard gates or other access controls shall be required for private tracts. Entry gates shall be set back from the near curb line of any public street to provide a minimum 100 feet of storage for entering vehicles to stack without interfering with through traffic. Minimum design criteria and required features for guard gates are shown below:



## **GUARD GATE NOTES:**

1. D=1' per Dwelling Unit Served, 100' Minimum (Multiple Lanes may be used to satisfy storage distance requirement.)

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