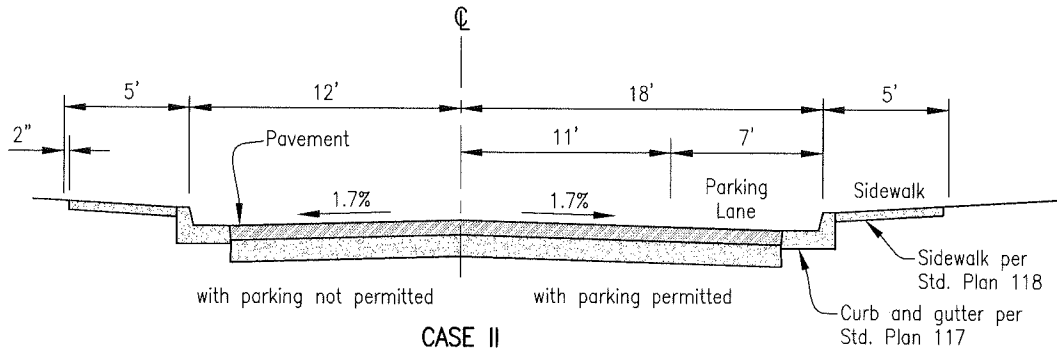
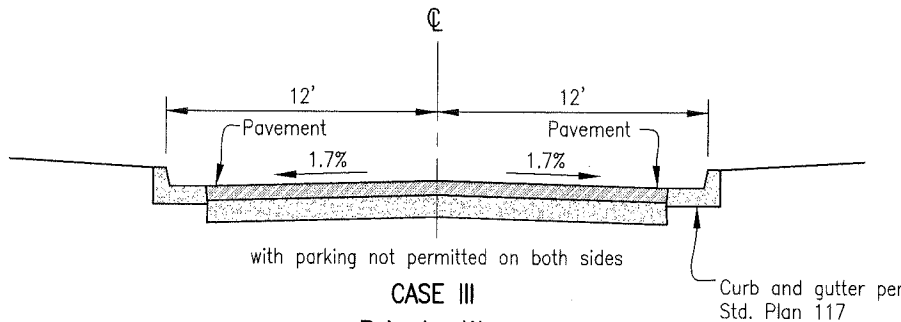


CASE I



CASE II



CASE III

Private Way

DEFINITION

1. A Private Way is a low-speed roadway for general circulation within a Townhome, Condo, and Apartment Development to access residential units, garage and parking areas. The maximum average daily traffic on a Private Way shall not exceed 850 trips.
2. For average daily traffic exceeds 850 trips and single family detached tract development, see Standard Plan 106.

NOTES

1. The design speed shall be 20 mph.
2. The minimum street width (curb-to-curb) shall be 28 feet in Fire Hazard Severity Zone (FHSZ)
3. The minimum structural section shall be 0.5' A.C. over 0.5' A.B. or determined by approved soil analysis
4. See Zoning Ordinance for dimensions of travel lane and parking lane when angled parking is used.
5. Curb return radius for Private Way to Private Way shall be 25 feet minimum.
6. Curb return radius for Private Way to Private Court shall be 25 feet minimum, or use Residential Driveway Apron.
7. The maximum dead-end length with turn-around shall be 1,000 feet measured from the centerline of an intersecting private way or street.
8. The minimum grade shall be 1.0% at the centerline unless otherwise approved by the City Engineer.
9. All pedestrian access locations shall conform to the latest Title 24 and Americans with Disabilities Act (ADA) requirements.
10. When a Private Way intersects with a Private Way or Public Street no parking shall be permitted within 20' of ECR or BCR on the Private Way.

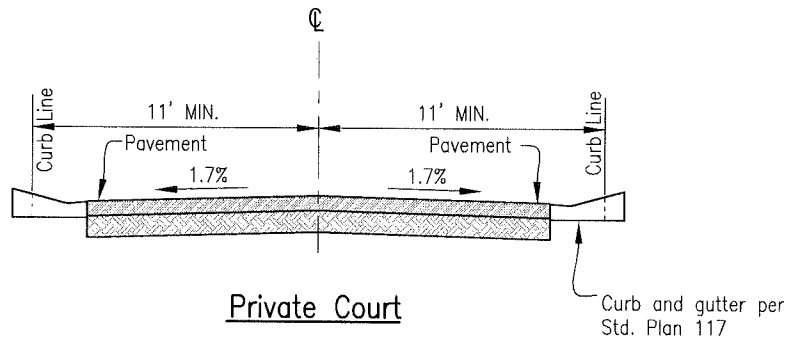
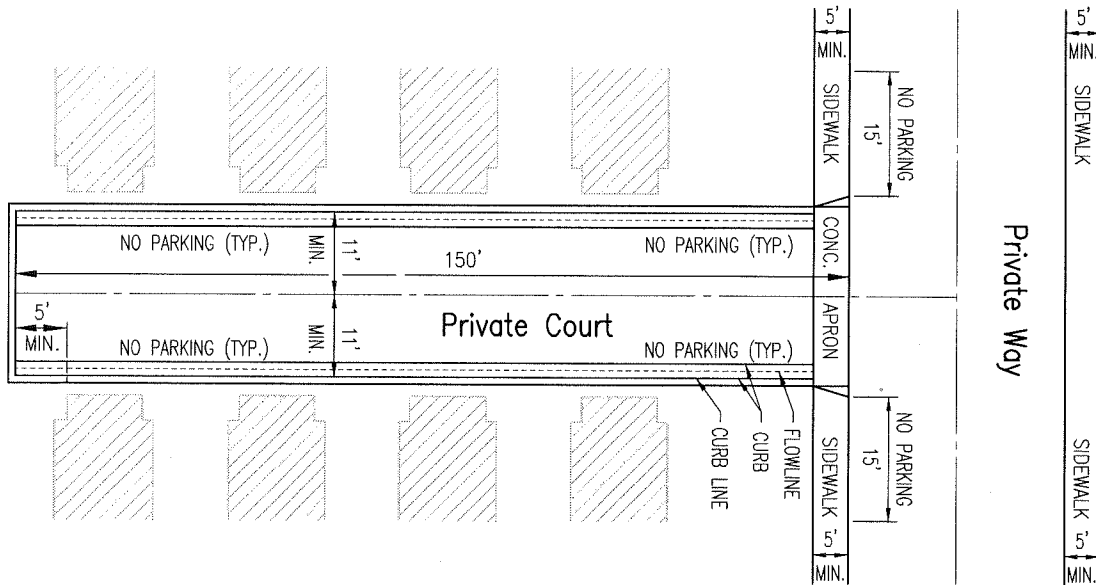


4/13/15
DATE APPROVED
PUBLIC WORKS DIRECTOR
CITY ENGINEER

CITY OF ORANGE PUBLIC WORKS DEPARTMENT

STREET SECTIONS FOR
TOWNHOME, CONDO, AND APARTMENT

STANDARD
PLAN No.
108A
SHEET 1 OF 2



DEFINITION

1. A Private Court is a low-speed, low volume, dead-end traveled-way within a Townhome, Condo, or Apartment Development to access residential garage and parking areas. The maximum average daily traffic on a Private Court shall not exceed 120 trips.
2. When the average daily traffic on the Private Court exceeds 120 trips, it shall be designed as a Private Way.

NOTES

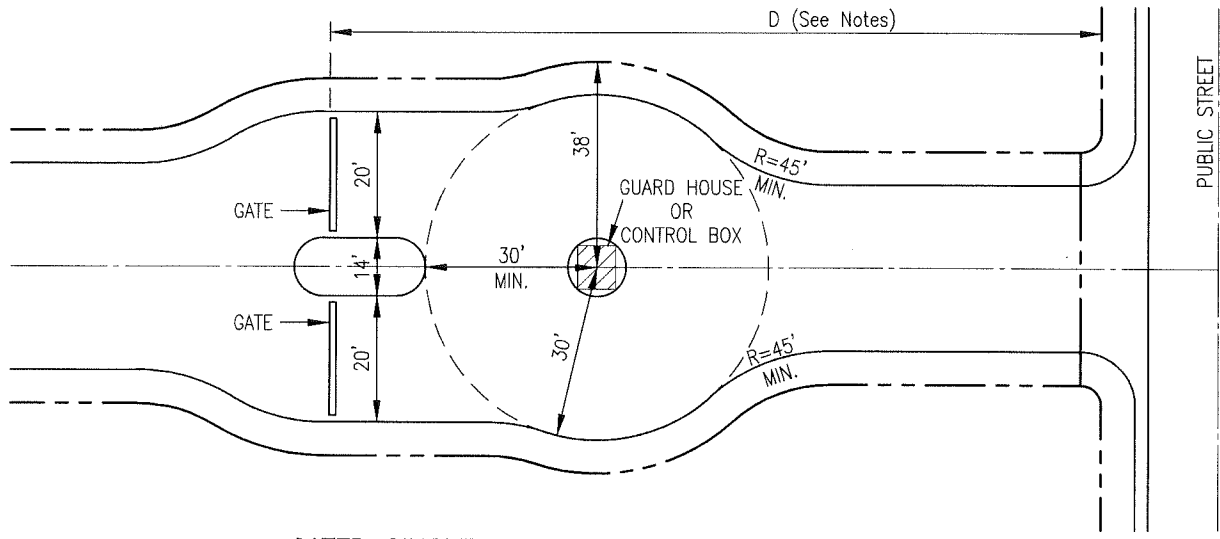
1. The maximum dead end length with no turn around shall be 150 feet measured from the curb face of an intersecting Private Way.
2. Private Court longer than 150 feet shall be designed to accommodate a minimum 38 foot outside vehicle turning radius.
3. The maximum dead end length with turn around shall be 320 feet measured from the curb face of an intersecting Private Way.
4. A minimum centerline distances between private courts shall be 75 feet.
5. The minimum structural section shall be 0.5' A.C. over 0.5' A.B. or determined by approved soil analysis
6. The minimum pavement cross slope shall be 1.7%.
7. The minimum grade shall be 1.0% at the centerline unless otherwise approved by the City Engineer.
8. Curb and gutter shall be constructed per Standard Plan 117, Type C
9. Sidewalk shall be provided in addition to the alley width when dwelling units also front the same alley.
10. All pedestrian access locations shall conform to the latest Title 24 and Americans with Disabilities Act (ADA) requirements.



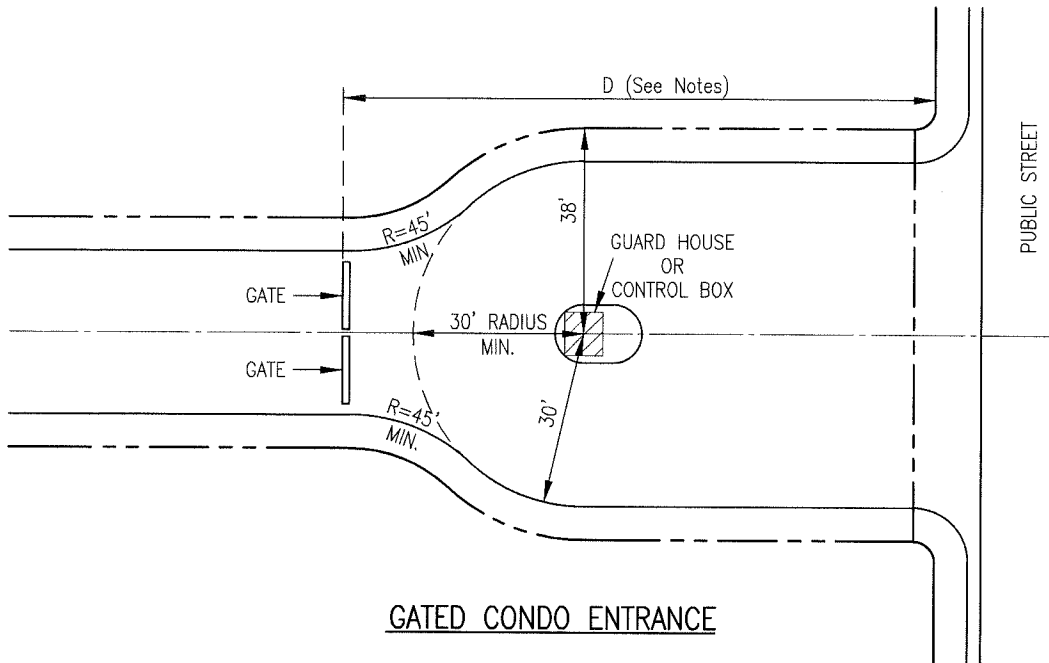
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STREET SECTIONS FOR
TOWNHOME, CONDO, AND APARTMENT

STANDARD
PLAN No.
108A
SHEET 2 OF 2



GATED SINGLE FAMILY DETACHED HOME ENTRANCE



GATED CONDO ENTRANCE

NOTES:

1. Entryways to private tracts shall be designed to emphasize their private status. Textured concrete or wide flare driveways, guard gates, or other access controls shall be required for private tracts.
2. "D" is the stacking distance measured from the public street right-of-way. It shall be determined by the City Engineer based upon the traffic volume of the adjoining public street and the size of development.



4/13/15
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CITY ENGINEER

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GATED ENTRANCE

STANDARD
PLAN No.
108B