

Metrolink Station Parking Structure Project

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The Orange County Transportation Authority (OCTA) is working to introduce more frequent rail service to Orange County. This will mean more trains and passengers using the Orange Depot station. The station, along with the OCTA bus terminal, is collectively known as the Orange Transportation Center (OTC). The Metrolink expansion will require approximately 900 parking spaces at the OTC, which currently has 375 spaces. Full roll-out of the Metrolink expansion is anticipated by 2030.

In early 2009, the City and OCTA entered into a cooperative agreement and began to plan for a new parking structure to serve the OTC.

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Orange Depot Area

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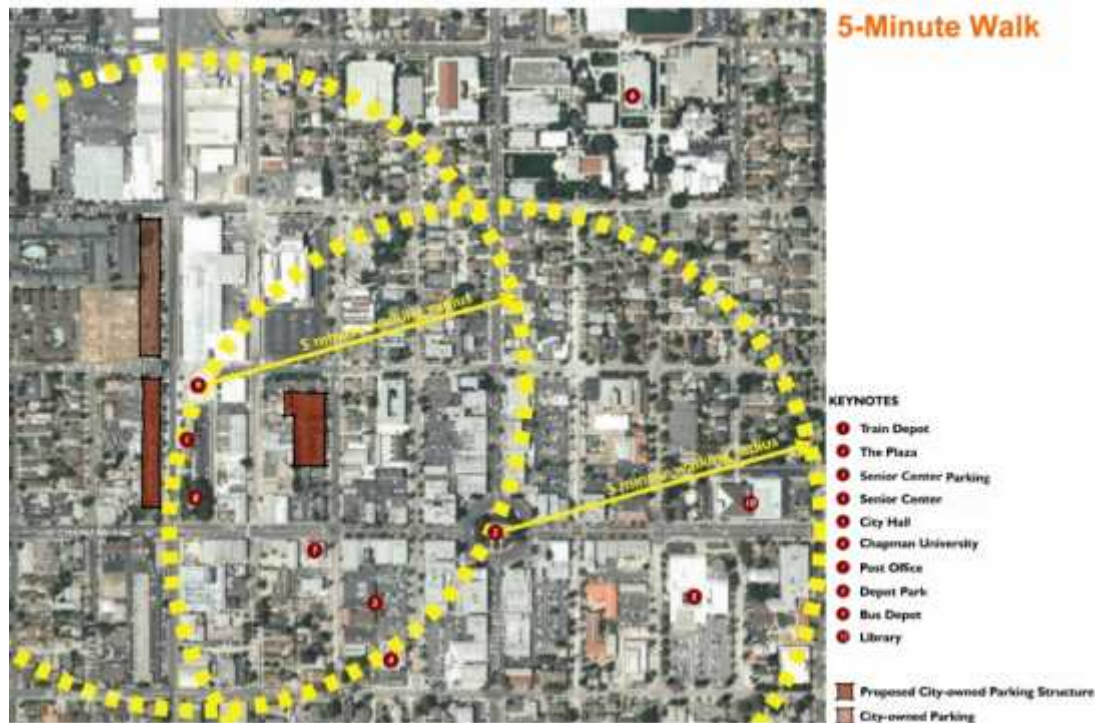
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The OTC is located in the City's Santa Fe Depot Area (Depot Area). The Depot Area is a distinctive element of the larger National Register-listed Old Towne Orange Historic District and encompasses several blocks surrounding the OTC. The concept of developing public parking near the OTC and linking those parking lots with the Plaza and the downtown commercial core has been the subject of several past planning efforts including the Depot-Plaza Pedestrian Connection Study (2008), the City General Plan (2010), and the Santa Fe Depot Specific Plan (2011). Initial parking structure planning efforts focused on identifying sites near the OTC within a 5 to 10 minute walking distance from the downtown commercial core.

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Initial Project Concepts

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The City and OCTA initially identified several options for expanded parking at the OTC. In November 2009, the Orange City Council identified a preferred design option which involved construction of two parking structures: one located on the Lemon Street parking lot at the northwest corner of Chapman Avenue and Lemon Street and one on the south end of the existing public parking lot located north of Chapman Avenue west of the train tracks. Ultimately, the parking structure on the West Chapman lot was found to be infeasible and the City's efforts were refocused on the Lemon Street site.

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Lemon Street Proposal

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This preliminary concept was identified as the preferred design for the Metrolink Parking Structure Project on the Lemon Street Parking Lot. The City Council reviewed this concept in November 2009 and directed staff to move forward with additional design work. On November 9, 2010, the Orange Redevelopment Agency Board approved an agreement with LPA, Inc. for architectural and engineering design services for the project. Design work for the Metrolink Parking Structure Project began in early 2011. At the time, the concept included residential units wrapping the parking structure and a retail/restaurant component. Later, the project was rescoped to focus only on the parking structure. The potential retail and restaurant elements are expected to come forward separately at a later date.

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Community Outreach

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In developing the preliminary design for the proposed parking structure, the City held a series of community meetings in 2010 through 2013. Preliminary design concepts were presented at these meetings. Community feedback was generally positive. There were questions and comments about how the parking structure would be operated, traffic impacts, the structure's height, lighting and light spill, the photovoltaic panels on the top deck of the structure, and the proposed brick treatment on the structure's exterior. Following the meetings, community feedback was carefully considered and the design concepts were further developed and refined. Additional community meetings are anticipated in summer 2015.

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Project Partners

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In addition to the Orange community, the City is coordinating with a number of government agencies to make the parking structure a reality including the Successor Agency to the Orange Redevelopment Agency (Successor Agency), Oversight Board to the Successor Agency (Oversight Board), State Department of Finance (DOF), Orange County Transportation Authority (OCTA), Federal Transit Administration (FTA) and State Historic Preservation Office (SHPO). Each of these agencies has a role in the project and is critical to the project's success.

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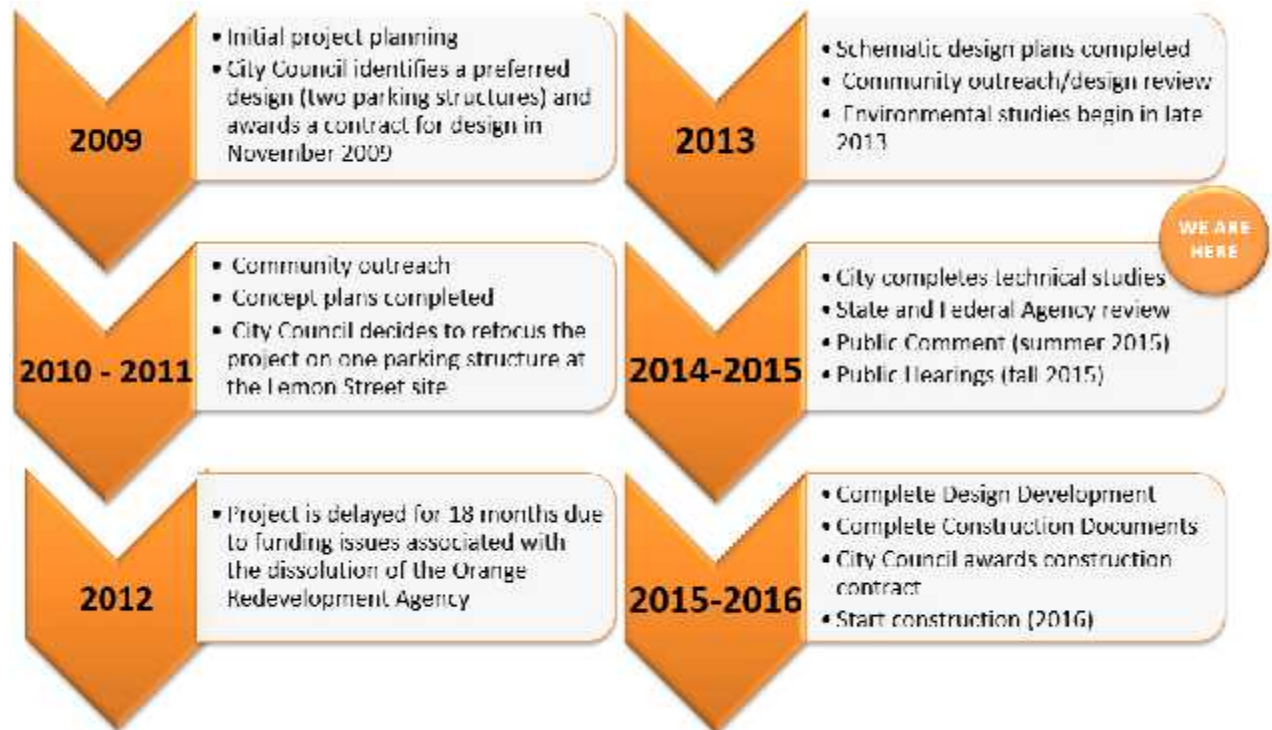


The Metrolink Parking Structure Project is funded with a combination of local, State and possibly federal funds. Project funding is primarily Measure M and State Transportation Improvement Program funds overseen by OCTA. A portion of the project is also funded by local Redevelopment Bond proceeds, which are overseen by the City's Successor Agency, Oversight Board, and the State Department of Finance. The project may also be federally funded in part by sources administered by FTA. In 2014, a Cooperative Agreement between the City, Successor Agency and OCTA was approved allocating \$16.7 million to the project through OCTA and \$3.7 million funded by the Successor Agency (subject to completion of design plans and environmental review). As part of the Redevelopment Agency wind-down, approval of Successor Agency funding decisions by the State Department of Finance is required. DOF approval was granted in June 2014. Click on the link for copies of the [Cooperative Agreement](#) and [DOF approval](#).

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There are many steps involved in bringing a project of this magnitude to fruition. The graphic above depicts major project milestones. The City has completed initial project planning, community outreach, and schematic design plans for the project. The environmental review process is currently underway. During this project phase, the City prepares studies evaluating the possible impacts of the project to the community and the environment. Once the studies are finalized, they are released to the public for a 30 day comment period. After, the studies and the project will be considered for approval at a series of public hearings. If the project is approved, the City will prepare final construction plans and begin construction in 2016. Construction of the project is expected to take approximately 12 months.

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Schematic Design

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This graphic shows the Metrolink Parking Structure project site. The northern portion is the parking structure site. The southern portion will be used for construction staging. The project will involve construction of a new five-level parking structure (two levels below grade, one level at grade and two levels above grade). The structure will provide 611 parking spaces: 500 for transit users and 111 general use spaces. The project also includes construction of an equipment enclosure, a bicycle plaza, flush-mounted photovoltaic panels on the parking structure's top deck and bike plaza canopy, and street and utility improvements to support the parking structure.

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This graphic shows a bird's eye view of the proposed Metrolink Parking Structure, looking northeast from Chapman Avenue at Cypress Street. The design of the structure draws from historic commercial and industrial buildings in the area.

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This graphic shows a perspective view of the proposed Metrolink Parking Structure looking northwest from Lemon Street at Chapman Avenue. The structure height would be up to 28 feet (with the elevator towers up to 41 feet tall) and would be clad with a brick veneer exterior wall finish to blend in with the historic commercial and industrial buildings in the area.

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Future Possibilities

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After construction of the parking structure, the southern portion of the site used for construction staging will be left as is. There are many possibilities for future use of the property. The property is zoned for mixed-use development. The graphic above shows what the site might look like from Chapman Avenue at Lemon Street if it were developed with retail and restaurant uses in the future.

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For more information or to be added to the project notification list, please contact:

Lisa Kim, Economic Development Manager

City of Orange – City Manager’s Office

Email: lkim@cityoforange.org

Office: (714) 744-2207

For information about the planning or environmental review process, please contact:

Jennifer Le, Acting Principal Planner

City of Orange - Planning Division

Email: jle@cityoforange.org

Office: (714) 744-7238

